



San Francisco Fire Department Bureau of Fire Prevention

Shared Spaces & Slow Streets

Fire Marshal Ken Cofflin

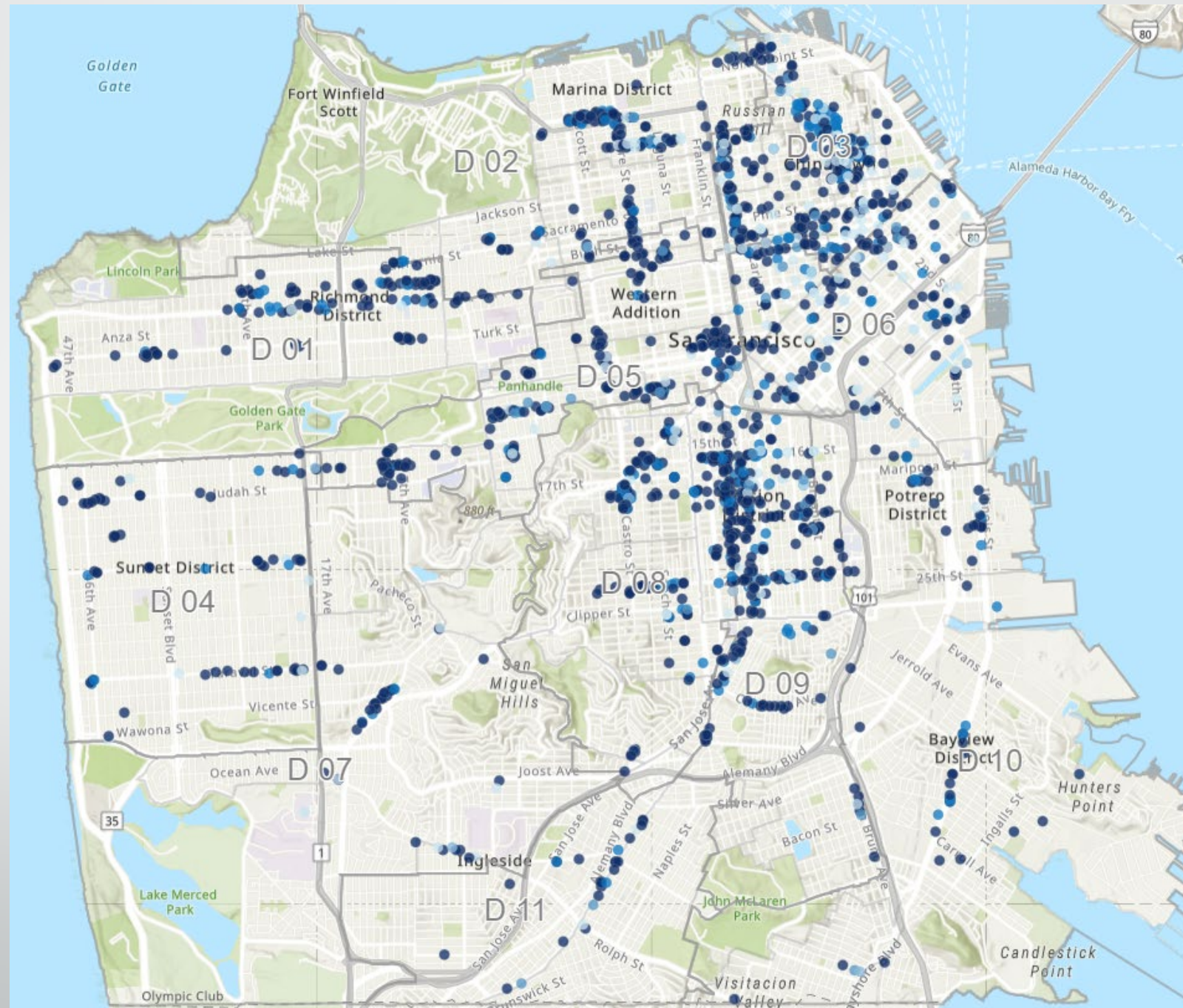
Shared Spaces

> **2000** – SFMTA Shared Spaces Permit Applications

> **1500** – Approved Permits

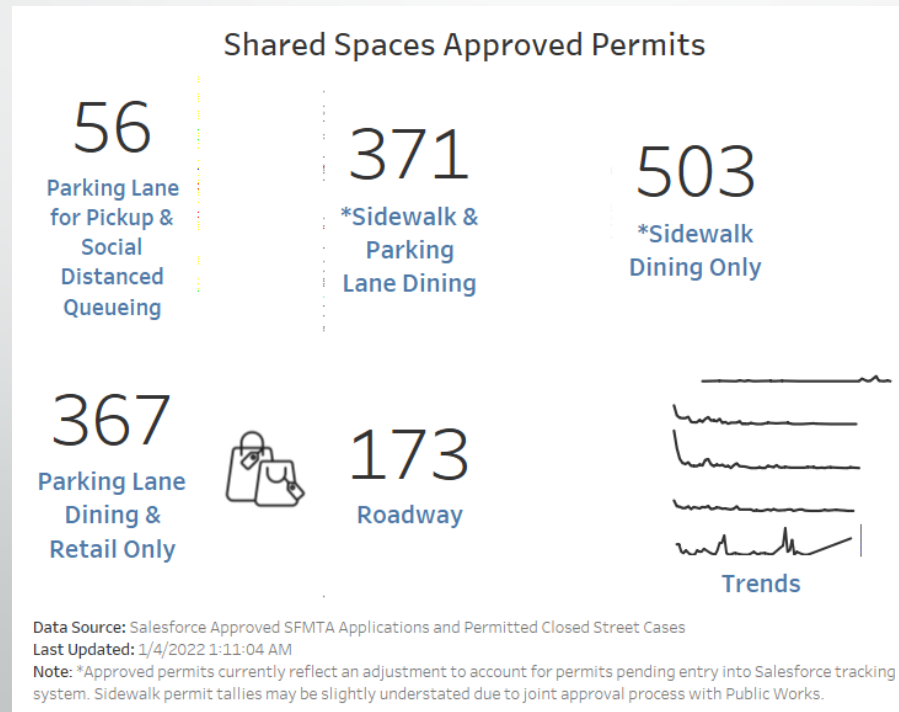
> **1100** – No. of Locations inspected by SFFD BFP Bi-weekly

≈ **750**- Restaurants Operating Curbside



What is Shared Spaces?

Shared Spaces is a multi-agency program of San Francisco's **Economic Recovery Task Force** that allows merchants and community groups to use sidewalks, full or partial streets, and other nearby public spaces like parks, parking lots and plazas, for restaurant pick-up and other neighborhood retail activities allowed under San Francisco's Public Health Orders.





SF Mayor Says Restaurants Can Take Over City Sidewalks, Parking Spots, and Streets

Bars and restaurants can apply for free, expedited permits to take public space over for takeout and (eventually) dining
by [Eve Batey](#) May 26, 2020, 11:52am PDT



MISSION LOCAL

local news for a global city

Car-free Valencia: On-street, outdoor dining approved for three-month pilot

by [Joe Eskenazi](#) July 16, 2020

Shared Spaces: a crisis-driven experiment that could permanently change SF's urban landscape

by [Hayden Manseau](#) September 22, 2020



KPIX CBS SF BayArea

SF Supes Vote To Keep Pandemic-Era Parklets Making Outdoor Dining, Bars Permanent

July 13, 2021 at 10:30 pm

Shared Spaces Program Development Timeline

March 15, 2020 – CA Governor orders bars closed and reduced capacity at restaurants due to COVID-19

May 26, 2020 – San Francisco Temporary Shared Spaces program launches to assist City businesses survive; expected to end by December 31, 2020

August 2020 – Bureau of Fire Prevention begins bi-weekly inspections of all City Shared Spaces for fire department operational access compliance

December 2020 – Temporary Shared Spaces program extended through June 30, 2021

March 12, 2021 – Mayor Breed introduces BOS legislation to make the Shared Spaces program permanent

July 20, 2021 – SF Board of Supervisors passes legislation making the Shared Spaces program permanent starting January 1, 2022, and thus ending the temporary program on December 31, 2021

August 26, 2021 – “Permanent” Shared Spaces program is active with intent to have compliant spaces Citywide by July 1, 2022

December 14, 2021 – Board of Supervisors introduces legislation to extend the “temporary to permanent” Shared Spaces conversion compliance date to April 1, 2023, and restricting compliance fines and fees on businesses until 2023.

Types of Shared Spaces

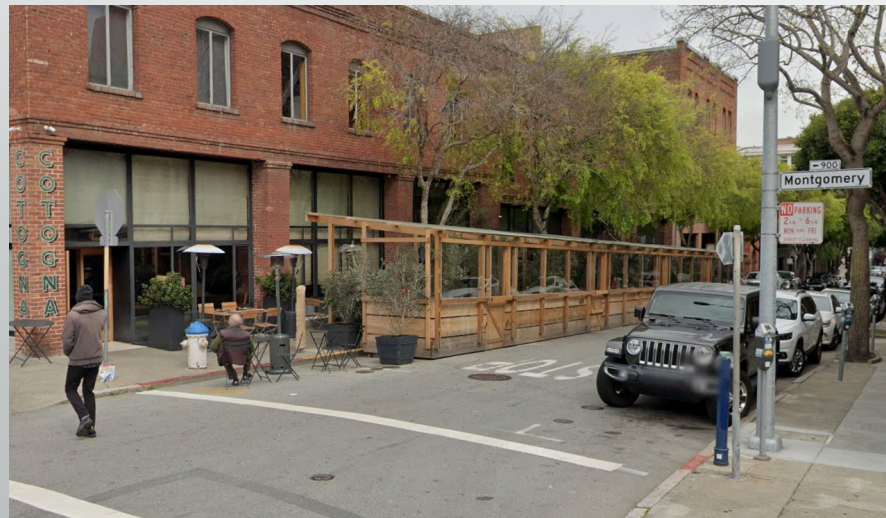
Sidewalk



Roadway



Parking Lane (Curbside)



Open Lots



List of currently permitted Shared Space street closures [Updated 1/05/2022]

Valencia Street - Mission

- Valencia Street from 16th to 17th streets; Valencia Street from 18th to 19th streets; Valencia Street from 20th to 21st streets
- Fridays, 4:00 p.m. to 11:00 p.m.; Saturdays, 1:00 pm to 11:00 p.m.

Galvez Avenue - Bayview

- Galvez Avenue at 3rd Street
- Saturdays, 7:00 a.m. to 3:00 p.m.

Austin Street - Lower Polk

- Austin Street from Polk Street to Van Ness Avenue
- Sunday through Tuesday, 10:00 a.m. to 10:00 p.m. each day; Wednesdays and Thursdays, 11:00 a.m. to 11:00 p.m. each day; Fridays and Saturdays, 10:00 a.m. to 11:59 p.m. each day

Natoma Street

- Natoma Street from 2nd Street to easterly terminus
- Monday through Friday, 10:30 a.m. to 8:00 p.m. each day; Saturdays and Sundays, 10:30 a.m. to 7:00 p.m. each day

Hayes Valley

- Hayes Street from Laguna to Gough streets; Octavia Street from Hayes to Ivy Streets
- Fridays, 4:00 to 10:00 p.m., Saturdays and Sundays 10:00 a.m. to 10:00 p.m. each day

Jane Warner Plaza - Castro

- Jane Warner Plaza
- Daily 11:00 a.m. to 6:30 p.m.

Roadway Shared Spaces

Larkin Street - Tenderloin

- Larkin Street from Eddy Street to O'Farrell Street
- Thursday through Sunday, 11:00 a.m. to 8:00 p.m. each day

37th Avenue - Outer Sunset Farmers Market & Mercantile

- 37th Avenue from Ortega to Quintara streets; Pacheco Street between Sunset Boulevard and 37th Avenue
- Sundays, 6:00 a.m. to 7:00 p.m.

37th Avenue - Sunset Wellness Mercantile

- 37th Avenue from Ortega to Pacheco streets
- Weekdays and Saturdays, varying hours between 6:00 a.m. to 9:00 p.m.

Ocean Avenue Parking Lot

- Ocean and Junipero Serra Parking Lot located at 2500 Ocean Avenue
- Closure: 24/7

Noe Street - Castro / Duboce Triangle

- Noe Street from Market Street to Beaver Street
- Sundays Only, 10:00 a.m. to 8:00 p.m. each day

Jessie Street - Westfield San Francisco Centre

- Jessie Street from 5th to Jessie West streets; Jessie West Street from Mission to Jessie streets
- Friday through Sunday, 3:00 p.m. to 10:00 p.m. each day.

Stevenson Street - SoMa

- Stevenson Street from 6th to 7th streets
- Tuesdays, 11:00 a.m. to 2:00 p.m.; Thursdays, 4:00 p.m. to 10:00 p.m.; Saturdays, 11:00 a.m. to 2:00 p.m.

12th Street - SOMA Second Saturdays

- 12th Street from Folsom Street to Bernice Street
- 2nd Saturday of the month, 9:00 a.m. to 6:00 p.m.

Trinity Street - RMW architecture + interiors

- Trinity Street Street from Bush Street to Sutter Street
- Daily, 8:00 a.m. to 10:00 p.m.

Single Business Sponsored Street Closures

Beach Street

- Beach Street (southernmost travel lane only) from Hyde Street to 210-foot westerly

- Closure: 24/7

Waverly Place

- Waverly Place from Clay to Sacramento Streets

- Thursday through Sunday, 5:00 p.m. to 11:00 p.m. each day

Jasper Place

- Jasper Place from Green to Union streets

- Monday through Friday, 5:00 p.m. to 11:00 p.m. each day; Saturdays, 11:00 a.m. to 11:00 p.m.; Sundays, 11:00 a.m. to 10:00 p.m.

Jack Kerouac Alley

- Jack Kerouac Alley from Columbus to Grant avenues

- Sundays, Mondays, and, Wednesdays, 11:00 a.m. to 10:00 p.m. each day; Tuesdays and Thursdays, 11:00 a.m. to 11:00 p.m.; Fridays and Saturdays, 11:00 a.m. to 2:00 a.m. each day

Leidesdorff Street

- Leidesdorff Street from Pine to California streets

- Daily 10:00 a.m. to 10:00 p.m.

Via Bufano

- Via Bufano from Greenwich Street to Columbus Avenue

- Friday through Wednesday, 1:00 p.m. to 11:00 p.m. each day; Thursdays, 4:00 p.m. to 11:00 p.m.

Steiner Street

- Steiner Street from Chestnut Street to Lombard Street

- Monday through Thursday, 5:00 p.m. to 10:00 p.m. each day; Fridays, 5:00 p.m. to 11:00 p.m.; Saturdays and Sundays, 10:00 a.m. to 11:00 p.m. each day

Leidesdorff Street

- Leidesdorff Street from Sacramento to Commercial streets

- Daily 10:00 a.m. to 10:00 p.m.

Ritch Street

- Ritch Street from Townsend to Brannan streets

- Daily 4:00 p.m. to 10:00 p.m.

Washington Street

- Washington Street from Fillmore to Steiner streets

- Daily 8:00 a.m. to 11:00 p.m.

Gold Street

- Gold Street from Montgomery to Balance streets

- Daily 4:00 p.m. to 12 midnight.

Ivy Street

- Ivy Street from Gough Street to Octavia Street

- Fridays, 2:00 p.m. to 9:00 p.m. each day; Saturdays and Sundays, 11:00 a.m. to 9:00 p.m. each **Michigan Street**

- Michigan Street from Marin Street to Cesar Chavez; Marin Street from Illinois Street to easterly terminus

- Fridays, 4:00 p.m. to 10:00 p.m.; Saturdays and Sundays, 2:00 p.m. to 8:00 p.m. each day

Maiden Lane

- Maiden Lane from Kearny Street to Grant Avenue

- Mondays through Wednesdays, 5:00 p.m. to 12 midnight; Thursdays and Fridays, 5:00 p.m. to 1:30 a.m.; Saturdays, 12 Noon to 1:30 a.m.; Sundays, 12 Noon to 12 Midnight each day

Harlan Place

- Harlan Place from 115-foot east of Grant Avenue to Easterly Terminus

- Sundays through Wednesdays, 4:00 p.m. to 10:00 p.m.

12th Street - The SF Eagle

- 12th Street from Bernice Street to Harrison Street

- Daily, 8:00 a.m. to 11:59 p.m.

Fern Street - Mayes Oyster House

- Fern Street from Polk Street to Van Ness Avenue

- Saturday and Sunday, 9:00 a.m. to 3:00 p.m. each day

Beckett Street

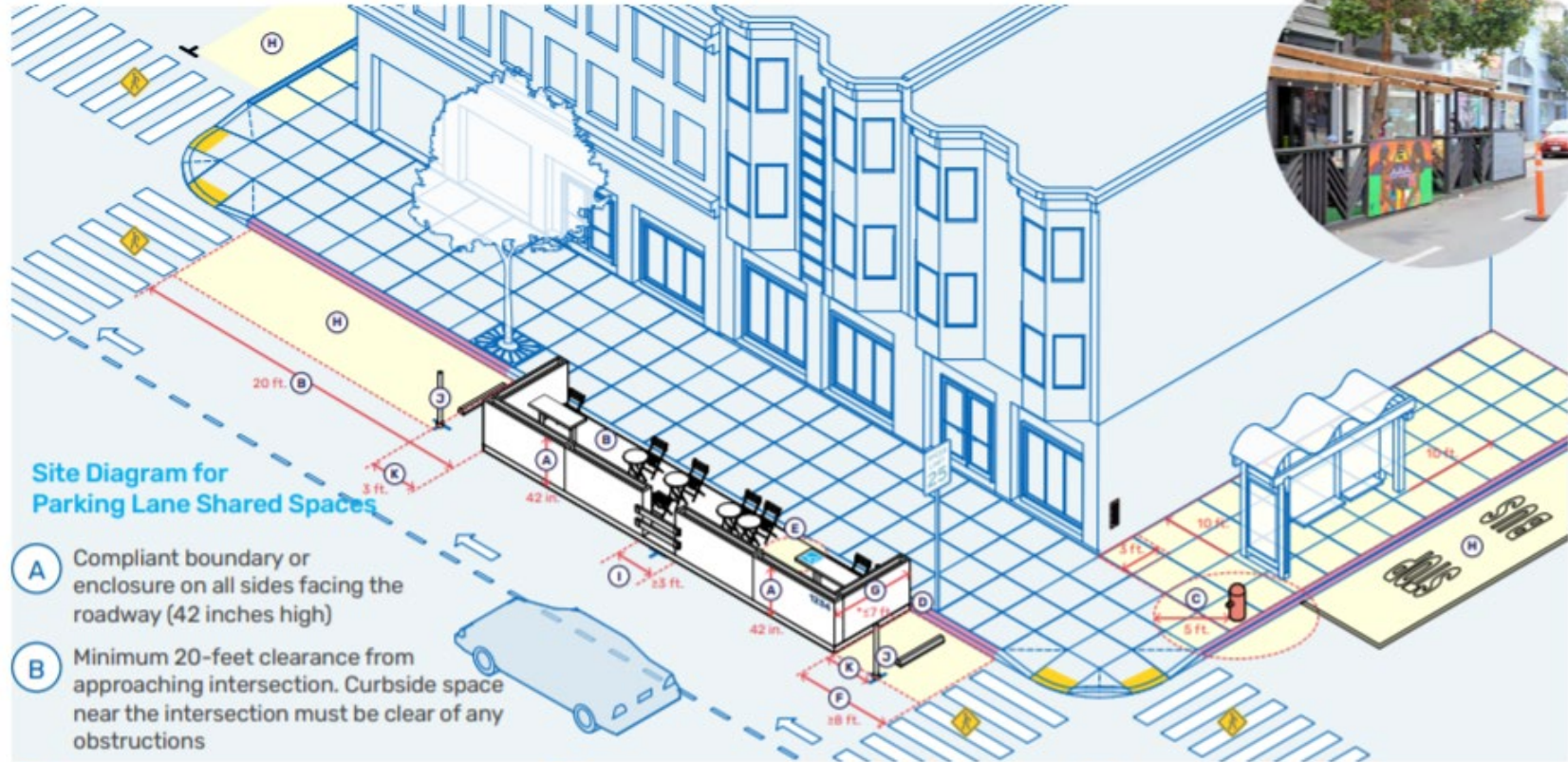
- Beckett Street from Pacific Street to Jackson Street

- Saturdays and Sundays, 10:00 a.m. to 11:59 p.m. each da

3 Types of Parking Lane (Curbside) Shared Spaces

- **Public Parklet** - a fixed structure providing full-time, publicly accessible space and no commercial activity.
- **Commercial Parklet** - a fixed structure where an operator uses the parklet for commercial activity during business hours with a bench or other public seating facility
- **Movable Commercial Parklet** - a space occupied by the operator using movable elements during limited business hours with a bench or other public seating facility, after which the curb space is used for loading or short-term parking.

Permanent Shared Spaces Design Guidelines



- A** Compliant boundary or enclosure on all sides facing the roadway (42 inches high)
- B** Minimum 20-foot clearance from approaching intersection. Curbside space near the intersection must be clear of any obstructions
- C** No structures permitted in the parking lane located within 5 feet of a fire hydrant.
- D** Parking Lane structures must end at sidewalk. No structures shall be fixed to the sidewalk except for platform thresholds or accessible ramps where permitted
- E** 30 inches x 48 inches clear space for wheelchair users at accessible tables & counters

- F** 8ft clearance when exiting an intersection
- G** Structure may occupy 7 feet **maximum** width of the parking lane for parallel parking, and 14 feet **maximum** width for angled or perpendicular parking (exceptions apply along rail, cable car)
- H** No structures permitted at active transit stops or accessible parking zones. No furniture shall be placed within 10 feet of a bus shelter.

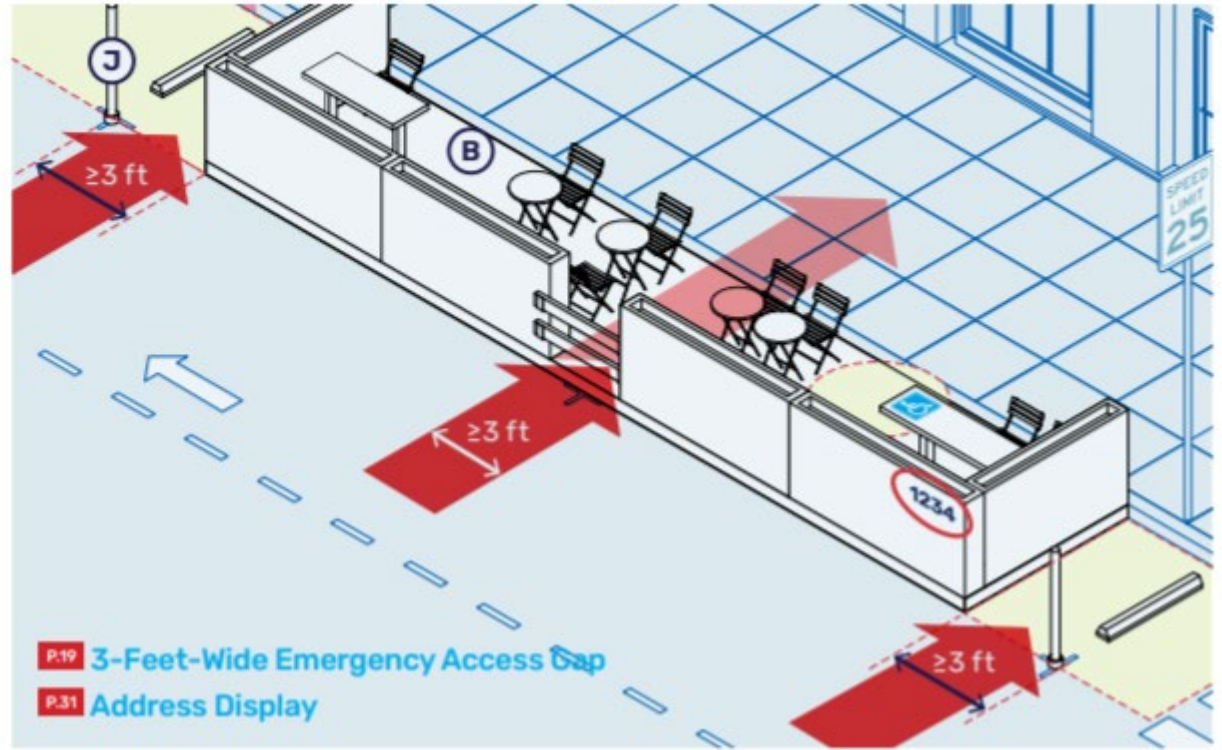
- I** **Minimum** 3-foot emergency access gap required for every 20 feet of structure, with vertical and horizontal clearance from street to building. Gap must be clear of obstructions like furniture, trees, heaters, meters, poles, etc.)
- J** Rubber wheel stops and soft-hit post are required at both ends of parklet (follow specifications described in this section)
- K** A 3-foot buffer is required at each end of the Shared Spaces structure

EMERGENCY ACCESS



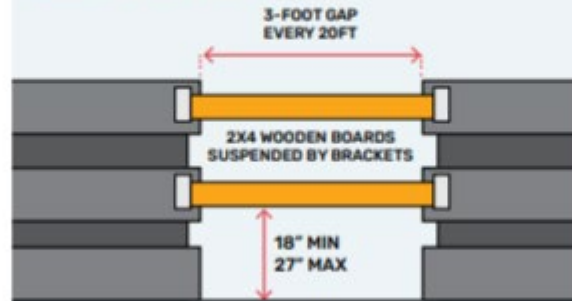
Emergency responders utilize the most direct path to access a building from the street to assist citizens in need of medical attention or for firefighting operations. Obstructing emergency access may lead to immediate danger to health or life safety. The following safety guidelines are required:

- P.19** A minimum 3-foot-wide emergency access gap, with vertical and horizontal clearance, is required for every 20 feet of structure length. This 3ft clearance shall be maintained at all times, including outside of business hours.
- P.20** A removable barrier is required at the 3-foot gaps. The barrier must be easily removable by emergency personnel at all times, and must be kept clear of any obstacles like tables, chairs, bike racks, trees, poles, or other elements, as well as barricading at night.
- P.21** At no time can parking lane occupancy obstruct emergency facilities (including, but not limited to fire hydrants, red zones, fire department connections, fire escapes, etc.).
- P.22** The sidewalk space between the curb and the building and each 3-foot-wide emergency access opening shall remain clear of overhead obstructions (string lights, canopies, decorations, heaters, wires, trees, poles, etc.) at all times, as ladders and other emergency equipment may need to be carried through the opening to the building.
- P.23** No fixed obstructions to the view of the adjacent building above 42 inches other than a roof or canopy at a minimum of 8 feet.

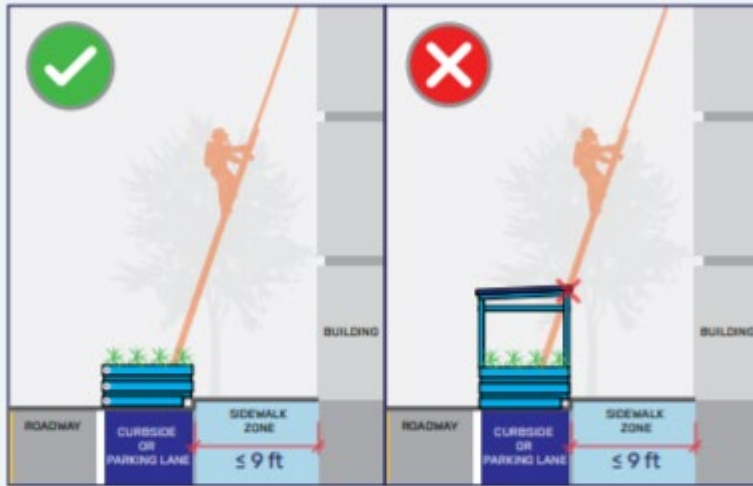


- P.19** 3-Foot-Wide Emergency Access Gap
- P.31** Address Display

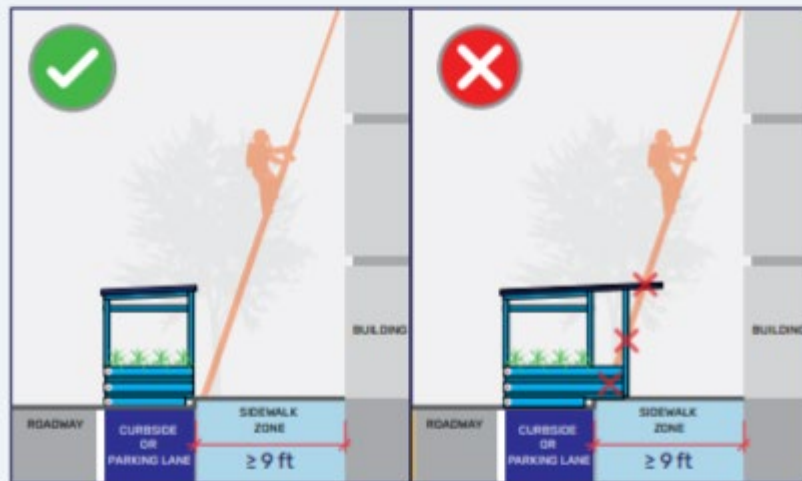
P.20 Removable Barrier



P.24 No canopies/roofs on parklets shall be permitted when sidewalk is less than 10 feet wide.



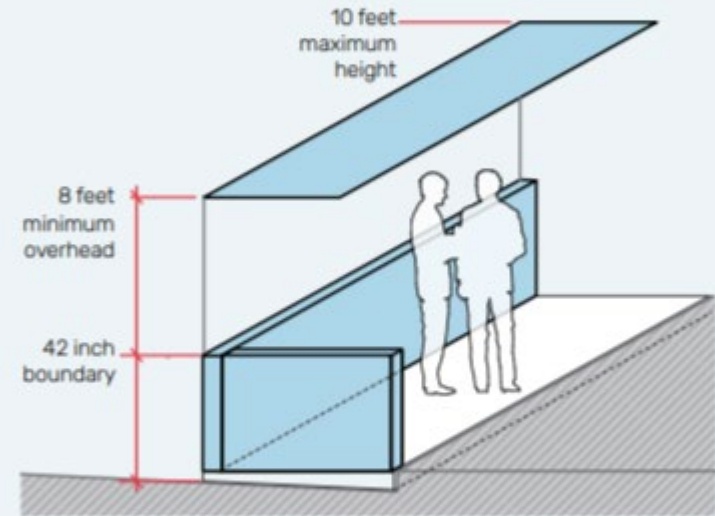
P.25 No part of the parklet structure shall extend over the curb or sidewalk.



P.26 No part of the structure shall exceed 10 feet in height, (including poles, posts, canopies, wires, string lights, signs, or pergolas) while still complying with the maximum 42 inch high enclosure construction requirements above.

If building a structure where Muni overhead lines are present, the top of the structure (including any roof) must not be taller than 9 feet above the road surface.

P.23 Overhead Structures/Canopies



P.27 Trees shall be maintained and trimmed to keep away from building, fire escape, and parklet roof/canopy. No tree shall be pruned without consent from Bureau of Urban Forestry. See *page 28* for more details.

P.28 Overhead fixed lighting or heater cords to fixed Parklets from buildings shall be plugged into a weatherproof electrical outlet installed on the exterior of the building at a minimum of 10 feet above the walking surface. No fixed cables/wires/conduit shall be used to support the light's cord and it shall be able to be easily unplugged by fire department personnel. Power cords between the building and parklet shall be limited to one (1) cord at each end of entire parklet space and shall not be adjacent, above, below or attached to any part of the fire escape. These

"flyover" cords shall be limited to standard cords plugged directly into an electrical outlet. No extension cords shall be permitted for fixed lighting or heaters. If unable to comply with the fixed lighting requirements, then electrical wires shall be installed in the sidewalk, below the walking surface with Public Works and Department of Building Inspection permits. Cable ramps are not permitted. Installation of weatherproof electrical boxes at your building require a DBI permit and inspection for approval.

P.29 All cables, cords, or wires used for lighting and speakers shall be run overhead. Cable ramps are not permitted. Portable Battery-Powered generators located within the shared space may be used.

P.29 Overhead cables, cords, or wires



P.30 Electric heaters may be used if applicant obtains an **electrical permit from the Department of Building Inspection** to install exterior electrical outlets at their building.

P.31 The address for each storefront or building where the outdoor dining area will be established shall be displayed at a height of 36 to 42 inches on the street-facing side of the barriers and be readily visible for emergency responders. Address numbers shall be a minimum of 4 inches tall (5/8-inch-wide stroke) with black numbers on a white background or other high-contrasting colors.

P.32 "NO PARKING - SFFD ACCESS - KEEP CLEAR AT ALL TIMES" signs shall be added to ends of parklet to keep motorcycles or bicycles from parking within the 3-foot end fire access gaps. Find signage specifications in *Section 4: Shared Spaces Operations*.

FIRE SAFETY

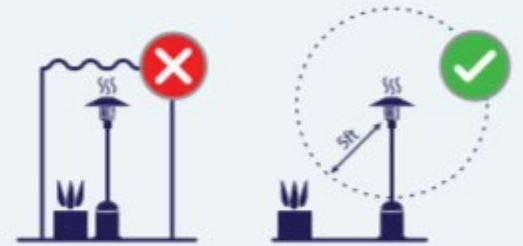


P.33 The use of propane (LPG) is regulated by the Fire Department. A separate SFFD Operational Permit is required for storage and use of liquid propane gas (San Francisco Fire Code 105.6.27) for Shared Spaces. Please go to www.sf-fire.org/permits or **628-652-3260** for more information.

P.34 Roofs and canopies are discouraged, but if installed, propane heaters cannot be placed underneath overhead canopies, awnings, inside buildings or tents and they must have 5-foot clearance from any flammable materials. A LPG permit will not be issued if roofs or canopies are constructed over the Shared Space.

P.35 A site plan with the desired location of each heater is required at time of SFFD permit application. A final on-site SFFD permit inspection will be conducted to ensure the

P.34 Propane Heater Clearance



proper spacing of heaters from combustibles and emergency access/egress.

P.36 Use outdoor heating fixtures safely and according to State and City Code Requirements.

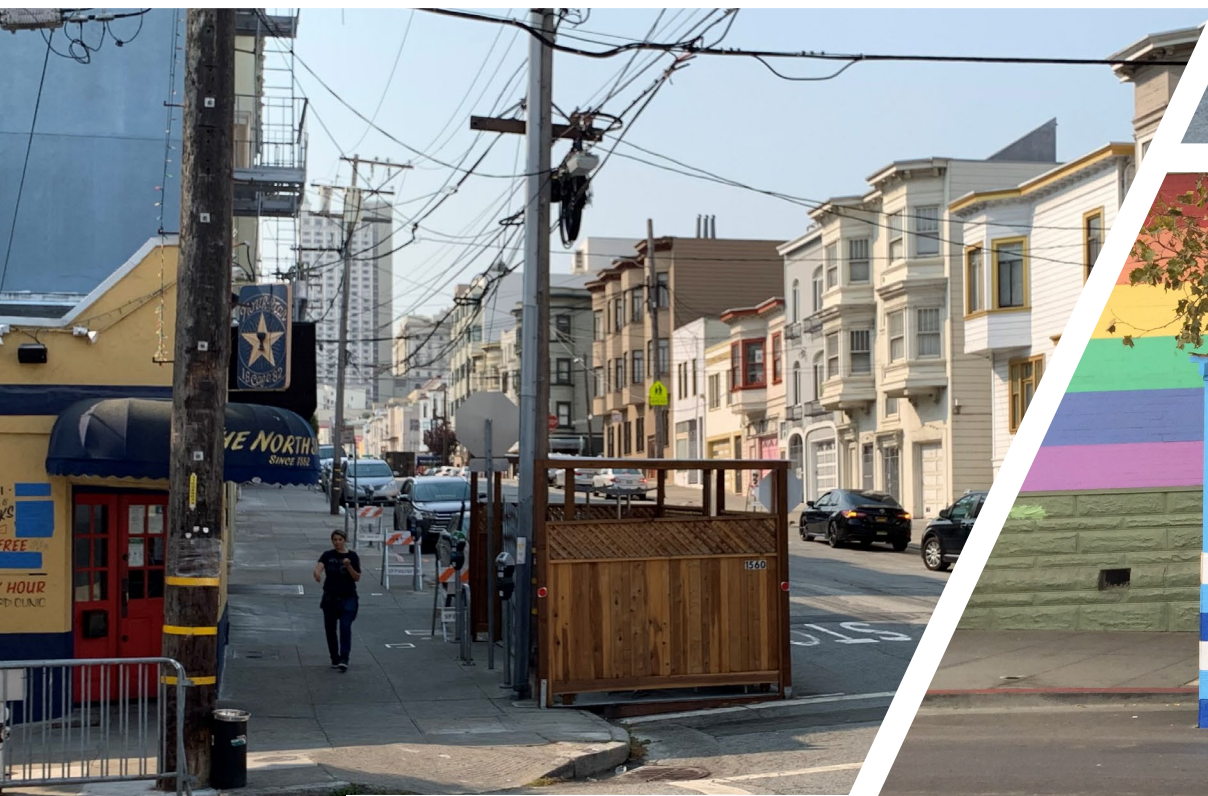
P.37 Propane (LPG) is flammable and can be readily ignited by a spark. If confined or exposed to fire, propane poses an explosion hazard. Propane is heavier than air, which means it accumulates in low places, where people and ignition sources exist.

P.38 On-site storage of 20-pound propane containers (5 gals.) is prohibited indoors.

P.39 Proposed outdoor storage of 20-pound propane containers requires plan review and inspection by SFFD. Propane (LPG) storage is not allowed in the public right-of-way, per Public Works.

P.40 If businesses do not have an approved private outdoor storage area, all propane containers must be removed from the premises at the close of business every day.

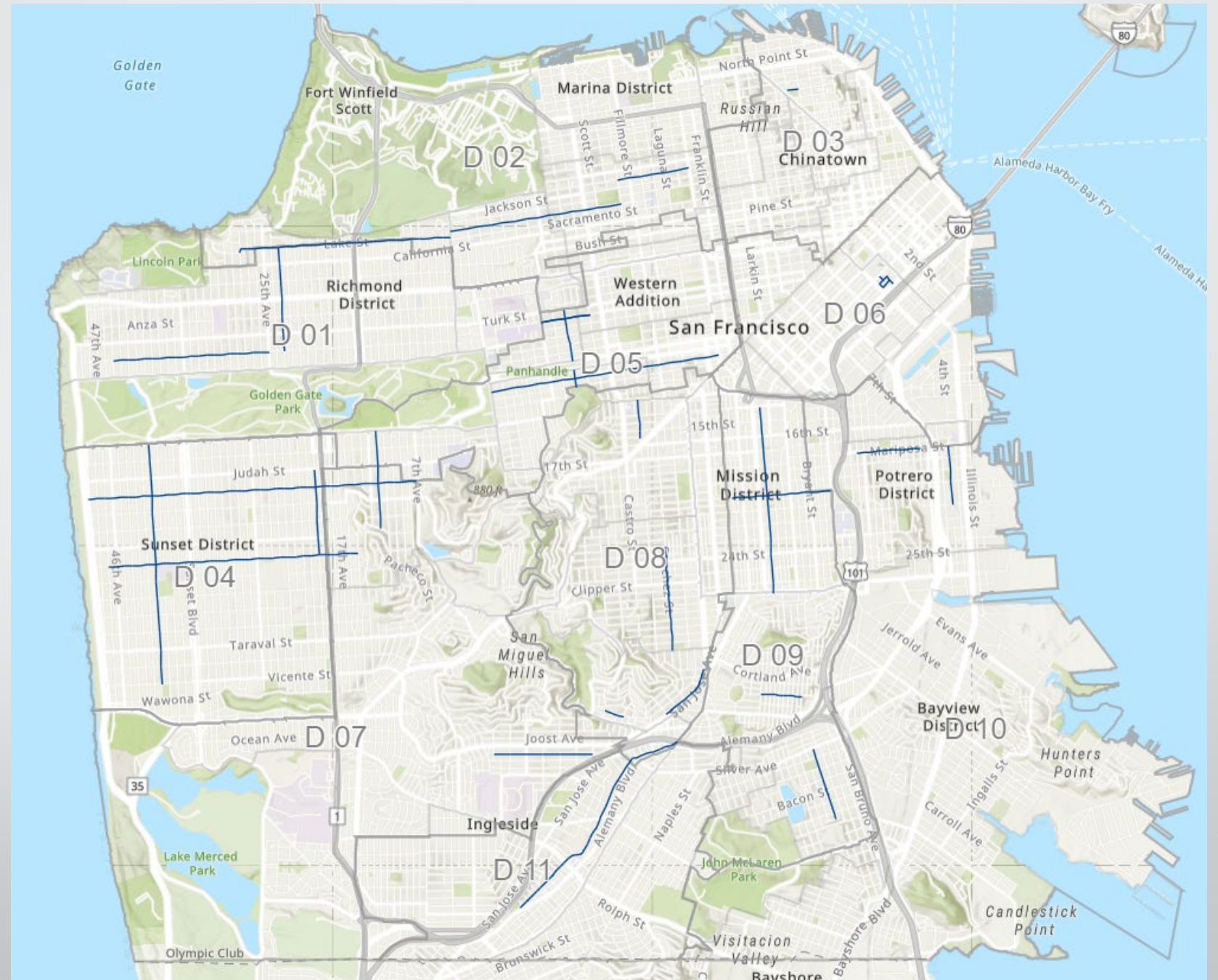
P.41 If natural gas heaters are to be used, the gas piping shall be installed below the walking surface in the sidewalk. Additional permits required.





SFMTA's Slow Streets Program

Pandemic &
Post Pandemic



What are Slow Streets?

- The SFMTA's Slow Streets program is designed to limit through traffic on certain residential streets and allow them to be used as a shared space for people traveling by foot and by bicycle.
- On these Slow Streets, signage and barricades have been placed to minimize through vehicle traffic and prioritize walking and biking.
- The goal of the Slow Streets program is to provide more space for socially distant essential travel and exercise during the COVID-19 pandemic.



Pandemic Slow Streets

1. **12th Avenue** from Lincoln Way to Noriega Street
2. **20th Street** from Lexington Street to Potrero Avenue
3. **Cabrillo Street** from 45th Avenue to 25th Avenue
4. **Clay Street** from Arguello Boulevard to Steiner Street
5. **Golden Gate Ave** from Masonic Ave to Broderick St
6. **Hearst Avenue** from Ridgewood Avenue to Baden St
7. **Kirkham Street** from 7th Avenue to 19th Avenue
8. **Lapu Lapu/Rizal/Tandang Sora/Bonafacio/Mabini** from Folsom Street to Harrison Street
9. **Lyon Street** from Turk Street to Haight Street
10. **Pacific Avenue** from Steiner Street to Gough Street
11. **Page Street** from Stanyan Street to Gough Street
12. **Shotwell Street** from Cesar Chavez to 14th Street
13. **Somerset Street** from Silver Avenue to Woolsey Street



...Continued

14. **20th Avenue** from Ortega Street to Judah Street
15. **23rd Avenue** from Lake Street to Cabrillo Street
16. **41st Avenue** from Lincoln Way to Vicente Street
17. **Arlington** from Roanoke Street to Randall Street
18. **Chenery Street** from Burnside Ave to Lippard Ave
19. **Kirkham Street** from 20th Ave to Great Highway
20. **Lake Street** from 28th Avenue to Arguello Blvd
21. **Lombard** between Mason Street to Powell Street
22. **Mariposa St** from Kansas St to Mississippi Street
23. **Minnesota** from Mariposa Street to 22nd Street
24. **Noe Street** from Duboce Avenue to Beaver Street
25. **Ortega Street** from 47th Avenue to 15th Avenue
26. **Sanchez Street** from 23rd Street to 30th Street



Post-Pandemic (Permanent) Slow Streets in Progress

(<https://www.sfmta.com/projects/post-pandemic-slow-streets>)

•Sanchez Street

- Evaluation: Completed
- Outreach/Design: [See the virtual open house](#)
- Authorization: [Approved August 3, 2021](#)
- Design Approval: [Approved September 10, 2021](#)
- Implementation: TBD

•Shotwell Street

- Evaluation: Completed
- Outreach/Design: [See the virtual open house](#)
- Authorization: [Approved August 3, 2021](#)
- Design Approval: [Approved September 10, 2021](#)
- Implementation: Partially implemented

Sanchez Slow Street Permanence

Virtual Open House

Brian Liang
March 9, 2021



Shotwell Slow Street Permanence

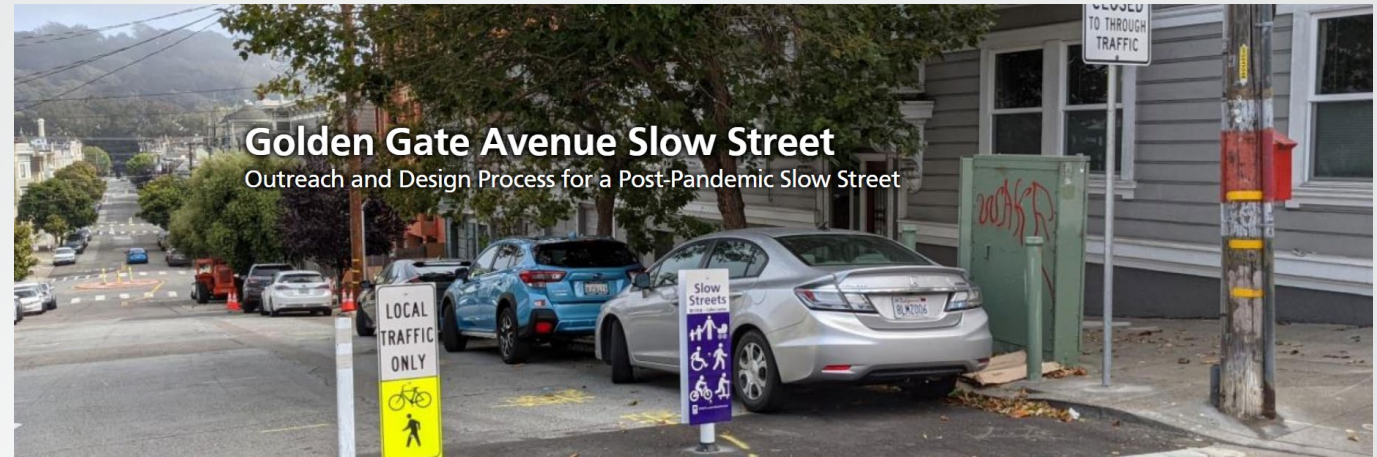
Virtual Open House

Brian Liang
March 8, 2021

Community Outreach

•Golden Gate Avenue

- Evaluation: Completed
- Outreach/Design: [See the project webpage](#)
- Authorization: [Approved August 3, 2021](#)
- Design Approval: TBD



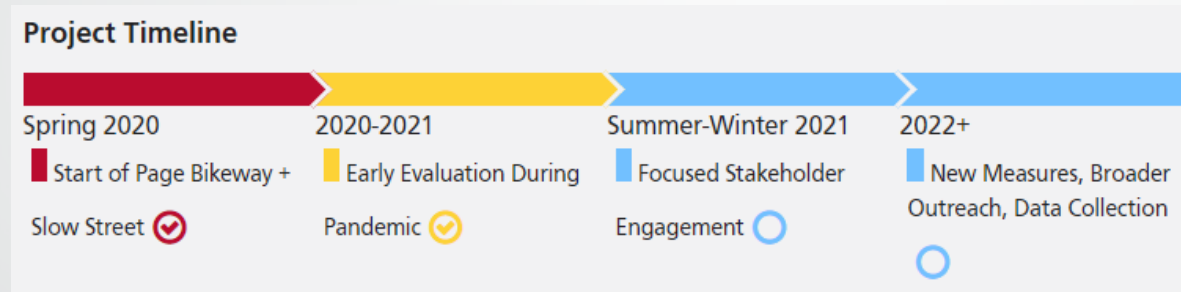
•Lake Street

- Evaluation: Completed
- Outreach/Design: [See the project webpage](#)
- Authorization: [Approved August 3, 2021](#)
- Design Approval: TBD

Page Slow Street

-Envisioning a post-pandemic Page Street

PROJECT STATUS (updated Fall 2021): In recognition of the many changes that have occurred on and around Page Street before and during the COVID-19 pandemic, staff are bringing together the 2020 Page Street Bikeway Improvements Pilot with COVID-era Slow Streets treatments into a new Page Slow Street project. The project team looks forward to engaging with the community in the months to come. In the meantime, please [sign up for our email list](#) to stay up-to-date on the latest.



Bringing Temporary Changes Together Into a New Pilot

The effects of COVID-19 on traffic patterns regionally have been significant and unpredictable, posing challenges for evaluating the original Page Street Bikeway Improvements pilot as planned. As the city's recovery is steadily progressing, the SFMTA hopes to advance outreach for changes on Page Street in 2022 that include:

- The 2020 Page Bikeway Improvements pilot (between Webster and Gough streets)
- Existing Slow Streets restrictions on Page Street that restrict non-local traffic (entire corridor to Stanyan Street)
- Formalized safety treatments and traffic diversion at signalized intersections (at Stanyan, Masonic, Divisadero)
- A framework for ongoing community art and placemaking along the corridor
- An evaluation focusing on broader neighborhood circulation, including on Haight, Oak, Fell, and side streets